

**Streets and Utilities Committee**  
**MEETING AGENDA**  
**Berryville-Clarke County Government Center**  
**101 Chalmers Court, Second Floor**  
**A/B Meeting Room**  
**Regular Meeting**  
**October 22, 2019**  
**10:30 a.m.**

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# Call to Order

# Approval of Agenda

# Unfinished Business

**Streets and Utilities Committee Agenda Item Report Summary  
October 22, 2019**

**Item Title**

Unfinished Business – Downtown street light pilot

**Prepared by**

Keith Dalton, Town Manager

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**Background/History/General Information**

On June 25, 2019, the Streets and Utilities Committee discussed lighting issues with officials from Rappahannock Electrical Cooperative. During that discussion, the Committee expressed its concern regarding lighting in Berryville’s Downtown, noting that well-lit areas are more inviting to customers. The Committee discussed improving lighting in a few locations in order to evaluate the change.

On July 9, 2019, the Town Council discussed street lighting and directed staff to provide a recommendation regarding a Downtown Street Lighting Pilot for review at the September 10, 2019 Town Council meeting.

The Town Council reviewed this matter at its September 10, 2019 meeting. The Council voted to proceed with a street lighting pilot.

Rappahannock Electrical Cooperative has informed staff that the lights have been ordered and that the installation should be complete by mid-November.

**Findings / Current Activity**

Lights have been ordered.

**Financial Considerations**

REC has agreed to install the lights identified in the Town Manager’s email of July 26, 2019 at no cost. If the pilot is expanded, then REC may decide to install the additional lights at no cost or may determine that it must charge for the additional work.

When higher wattage lights are installed, the monthly tariff for the lights in question will be adjusted.

**Schedule/Deadlines**

Installation is expected by mid-November.

**Other Considerations**

None

**Attachments**

None

**Recommendation**

None

**Sample Motion**

No action needed.

**No Attachment**

# New Business

## Streets and Utilities Committee Agenda Item Report Summary

October 22, 2019

### Item Title

Street Sign Specifications Town of Berryville Construction Standards and Specifications Manual

### Prepared By

Christy Dunkle and Chief Neal White

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### Background/History/General Information

The Town of Berryville Construction Standards and Specifications Manual establishes regulations for design and construction of public facilities being built for dedication to the Town of Berryville. Modifications to Section 10 Work on State Highways & Town Streets are included in this staff report which establish street sign specifications. Signage includes public and private street name signs and those signs used for enforcement and warnings. Updates are proposed for the materials specification supplement included with this report.

### Findings / Current Activity

Police, Public Works, Planning, and Administrative staff have been discussing the need for consistent street signage in the Town. With several new subdivisions coming online within the next year, staff is recommending the modified Chapter 10 of the Construction Standards and Specifications Manual included in this report. Identified in bold in the attached draft, proposed changes include the addition of regulations for street signage (e.g., lettering sizes, sign color and size, lettering specifications); mounting requirements; and other applicable signage specifications. A reference to this information is also proposed under Article IV Subdivision Design Standards, Section B.12. Street signs and names, identified in bold on page 20 of the attached portion of the Town of Berryville Subdivision Ordinance.

### Financial Considerations

Costs for replacement of existing signs have not been determined. Developers are responsible for providing and installing street signage upon development of the respective subdivision.

### Schedule/Deadlines

Staff is recommending that the Committee discuss the proposed draft and forward to Town Council for consideration at their November 12, 2019 meeting.

### Attachments

- Draft Section 10 Work on State Highway and Town Streets
- Draft street signage materials specifications
- Proposed modification to Town of Berryville Subdivision Ordinance referencing Section 10 and supplemental materials specifications

### Recommendation

Forward to Town Council for consideration.

## Section 10

### *Work on State Highways*

#### *I. General*

- A. The Virginia Department of Transportation (VDOT) maintains primary roadways (Business Route 7 and US 340) within the Town of Berryville Corporate Limits.
- B. The Contractor shall assume all responsibility for fulfilling any and all requirements specified herein and under the permit(s) obtained from the VDOT for work to be performed on state highways.
- C. All applicable provisions as established by the "Land Use Permit Manual, Commonwealth of Virginia - Department of Highways and Transportation, Richmond, Virginia; 1983," and subsequent revisions thereto, shall apply to this work, including provisions for revocation of permit.
- D. All work done under this permit within the road right-of-way shall, in all respects including location, alignment, elevation and grade; manner of performing the work; highway crossings; restoration and conditions; etc., be subject to VDOT direction and shall be done to the satisfaction of the Department.

### *Work on Town Streets*

#### *II. General*

- A. The Town of Berryville maintains public secondary streets within the Corporate Limits. **Proposed secondary streets located within Annexation Area B will also comply with regulations established by these standards and specifications once development occurs.**
- B. The Contractor shall assume all responsibility for fulfilling any and all requirements specified herein and under the permit(s) obtained from the Town of Berryville for work to be performed on Town street rights-of-way.
- C. All applicable provisions as established by the "Land Use Permit Manual, Commonwealth of Virginia - Department of Highways and Transportation, Richmond, Virginia; 1983," and subsequent revisions thereto, shall apply to this work, including provisions for revocation of permit.
- D. All work done under this permit within the street right-of-way shall, in all respects including location, alignment, elevation and grade; manner of performing the work; highway crossings; restoration and conditions; **installation of street name signs;** etc., be subject to Town of Berryville Department of Public Works direction and shall be done to the satisfaction of the Department.

### *III. Street Name Sign Specifications*

#### **A. Panel / Plaque Standards**

- a. Street name signs will conform with guidelines established by the Manual of Uniform Traffic Control Devices (MUTCD) and associated manuals or papers referenced within the MUTCD. These documents will be reviewed on a routine basis to ensure this standard and specifications are aligned.
- b. The standard street name sign shall be the MUTCD D3-1 series with a green background, rounded corners with a white border and lettering. The shade of green for the background shall be reflective 3M green or an approved equivalent. The private street name sign shall be the MUTCD D3-1 series with a brown background, rounded corners with a white border and lettering. The shade of brown for the background shall be reflective 3M 3430 GP 3430 brown or an approved equivalent.
- c. All signs will be constructed from flat aluminum sheet material with a thickness of 0.80" and will comply with retroreflective standards established in the MUTCD (sec. 2A.07).
- d. The standard post mounted street name sign shall measure 6" vertically. Street name signs mounted overhead shall measure 24" vertically.

#### **B. Lettering**

- a. Signs shall be lettering utilizing Series D 2000 as illustrated in the Standard Alphabets for Traffic Control Devices as published by the Federal Highway Administration (2000).
- b. For the standard post mounted sign, uppercase letters will be 4" in height with lower case letters being 3" in height. Overhead mounted signs will utilize uppercase letters 12" in height and lower case letters 9" in height.
- c. Street name lettering will begin with an uppercase letter followed by lower case.
- d. All directional prefixes will be a single uppercase letter. As an example, East Main Street would be designated as E Main St.

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 Section 10 \* Work on State Highways & Town Streets

- e. All roadway suffix designations will be standard abbreviations with the first letter capitalized followed by lower case letters.
- f. Letter spacing requirements will follow the Series D 2000 Standard Alphabets Spacing Chart (Standard Alphabets for Traffic Control Devices).

### C. Mounting

- a. Post mounted signs shall utilize a 2" square sign post made of 14 gauge aluminum with a FHWA approved breakaway support system conforming to ASSHTO standards and specifications for highway signs. The sign post for street name signs shall be gray in color.
- b. Overhead mounted signs shall comply with standards established by ASSHTO in Specifications for Structural Supports of Highway Signs, Luminaries, and Traffic Signals.
- c. For signs located in areas where pedestrian movements are likely to occur or on street parking is permitted, the height of the lowest portion of the sign panel to the finished surface shall have a minimum clearance of 7' and maximum clearance of 8'. The Director of Public Works may approve a modification to the maximum clearance height if conditions exist to necessitate a deviation.
- d. Unless otherwise approved by the engineer, the lateral clearance to the sign shall be a minimum of 2" from the face of curb or 4' from face of barrier if present.
- e. Post mounted signs will be attached to the top of the post using a 5.6" sign slot bracket, 5.6" sign T slot bracket and set screws. Additional information concerning the hardware specifications can be found in the Materials Specifications section of this document.

### IV. Other Street Sign Specifications

#### A. Plaques

- a. All other required highway signs will follow the standards and requirements established within the MUTCD.

#### B. Mounting

- a. Mounting materials shall be the same as defined in sec. 10 III C, with the following exceptions:

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Section 10 \* Work on State Highways & Town Streets

- i. Enforcement and warning signs will utilize a post yellow in color.
  - ii. Stop signs will utilize the yellow post and additional red retroreflective tape attached to the post in accordance with section 2A.21(04) of the MUTCD.
- b. Overhead mounted signs shall comply with standards established by ASSHTO in Specifications for Structural Supports of Highway Signs, Luminaries, and Traffic Signals.
- c. For signs located in areas where pedestrian movements are likely to occur or on street parking is permitted, the height of the lowest portion of the sign panel to the finished surface shall have a minimum clearance of 7' and maximum clearance of 8'. The Director of Public Works may approve a modification to the maximum clearance height if conditions exist to necessitate a deviation.
- d. Unless otherwise approved by the engineer, the lateral clearance to the sign shall be a minimum of 2" from the face of curb or 4' from face of barrier if present.
- e. No post will extend vertically over the posted sign or plaque greater than 6". All square posts will be capped with a pyramid style rain cap in a color matching the post.

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Section 10 \* Work on State Highways & Town Streets

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STREET SIGNAGE \* October 2019**DRAFT**

Items for Street Signage	Manufacturer	Model #	Specifications
Standard public street name sign	N/A	N/A	MUTCD D3-1 3M Green background, white letters white rounded corners
Standard private street name sign	N/A	N/A	MUTCD D3-1 3M3430EGP3430 brown background, white letters white rounded corners
Perforated traffic post	Trinity Highway	SQR-LOC	2" square 14 gauge Aluminum, FHWA approved breakaway support
Enforcement and warning sign posts	N/A	N/A	2" square 14 gauge galvanized yellow post
Sign slot hardware	N/A	N/A	Standard sign 5.6" sign slot
Sign T slot hardware	N/A	N/A	5.6" t slot

#### IV. SUBDIVISION DESIGN STANDARDS

##### A. GENERAL

The quality of a community is dependent on the quality of the individual subdivisions that are a part of it. A high-quality community is made possible by a well-designed infrastructure, which requires the cooperation of each subdivider and developer of land. Therefore, the design of each subdivision in the Berryville Area shall be prepared in accordance with the principles and recommendations established by the Berryville Area Plan for land use, traffic circulation, community facilities and public services, and in accordance with the following general principles:

1. Size of lots and blocks  
The size of lots and blocks and other areas for residential, commercial, industrial, and public uses shall be designed to provide adequate light, air, open space, landscaping, and off-street parking and loading facilities.
2. Solar access and preservation of natural features  
The arrangements of lots and blocks and the street system shall be designed to protect solar access and to make the most advantageous use of topography and natural physical features. Tree masses and large individual trees shall, whenever possible and consistent with the provisions of this Ordinance, be preserved. Any system of sidewalks and roadways and lot layout shall be designed to take advantage of the visual qualities of the area.
3. Materials and construction techniques
  - a. It is the intent of this Ordinance that all plat work, design, and construction work, normally done by Surveyors, Engineers, and Contractors, be performed in accordance with good standard practices, whether specifically addressed in this Ordinance or not. Such "standard practice" shall refer not only to work performed in the installation of necessary improvements and facilities, but also to the work of restoration of existing features and underground facilities.
  - b. All construction shall conform to the standards of the Town or, in the event no Town standards exist, to the standards of the Virginia Department of Transportation. Unless otherwise shown, all material and construction technique specifications shall be in accordance with the requirements of the Virginia Department of Transportation in effect at the time of the plan and specification submission.
  - c. All other design criteria and construction standards shall be in accordance with applicable Town Design and Construction Standards. Where standards and criteria are not provided or are found not applicable, the Director of Public Works and/or Town Manager shall provide the governing standards or shall rule upon those standards proposed by the developer.

Design Standards

In furtherance of the purposes of this Ordinance, the following minimum subdivision design standards, as applicable, shall be required and delineated on final subdivision plats.

B. STREETS1. Street standards and design

- a. All street and highway construction standards and geometric design standards shall be in accordance with those specified by the Virginia Department of Transportation and applicable Town Design and Construction Standards. In certain cases, the Town may modify street geometric design standards for local, collector, and minor loop streets with the provision that sufficient off-street parking be provided to complement the street system and approval for modification is obtained from the Virginia Department of Transportation where applicable.
- b. Specifications for improvements to proposed and planned streets shall be in accordance with the Virginia Department of Transportation and/or Town design standards and criteria established by the Town.
- c. All subdivisions must have direct access to publicly dedicated and publicly maintained roads, except that private roads may be permitted in accordance with the provisions in this Ordinance.
- d. Subdivisions shall be designed so that lots will not front any arterial road unless the physiography, shape or size of the tract would preclude other methods of providing access.

2. Street classifications

The classification of proposed streets shall be determined by an estimate of the anticipated vehicular traffic volume as currently prescribed, or as revised, by the Virginia Department of Transportation and shall apply to streets proposed by a subdivider and to all streets shown on the transportation element of the Berryville Area Plan.

3. Street layout

Streets in predominantly residential subdivisions shall be designed to discourage through traffic, but offset or jog streets shall be avoided.

4. Street width

- a. The right-of-way width for all public streets shall conform to the widths designated on the transportation element of the Berryville Area Plan.
- b. The right-of-way width for streets shall be not less than 50 feet.

5. Street right-of-way lines

Right-of-way lines shall conform to the property lines of lots and shall be parallel to the street center line.

6. Street grades

- a. The grade of streets shall not exceed 8 percent unless approved by the Town Council, and in no case shall a street grade exceed 10 percent.
- b. A minimum street grade of 0.5 percent shall be required.

Design Standards

7. Street approach angle  
Streets shall intersect at near right angles of not less than 80 degrees, unless otherwise approved by the Town Council or its agent, or upon recommendation from the Virginia Department of Transportation for specific reasons of contour, terrain, or matching of existing patterns.
8. Curb and gutter requirements  
Curb and gutter shall be required as provided in Article III Section 320 of the Berryville Zoning Ordinance. All curb and gutter sections shall be constructed in accordance with Virginia Department of Transportation Standards. (9/05)
9. Half-street sections  
Half-street sections (streets of less than the full right-of-way required) along the property line of land proposed for subdivision shall not be permitted, unless approved by the Virginia Department of Transportation. When a new subdivision abuts one side of an existing or platted street, the subdivider shall dedicate at least half of the right-of-way necessary to make such street comply with the minimum width fixed for the same by this section. However, where half-streets exist on adjoining property, the provisions of the section may be satisfied by the dedication of the remaining required right-of-way upon which the subdivision in question abuts. No building shall be permitted without such dedication.
10. Cul-de-sacs  
Cul-de-sacs (minor neighborhood streets designed to have one end permanently closed) shall not be longer than 600 feet. All cul-de-sacs must be terminated by a turnaround having a right-of-way radius of 50 feet and a paved radius (face of curb to face of curb) of 43 feet.
11. Coordination and continuation of streets adjacent to subdivision
  - a. Subdivision street layout shall allow for the planned continuation of existing streets in adjoining areas, and must not be such as to cause unnecessary hardship to owners of adjoining property which may be susceptible to future subdivision planning.
  - b. Subdivision streets shall be provided and designed to give access to adjoining acreage in conformance with the Berryville Area Plan and to the satisfaction of the Town.
  - c. Any proposed street that will extend an existing street shall be improved in like manner as the existing street unless as otherwise directed by the Virginia Department of Transportation (VDOT). The type of improvement and the construction materials shall be in accordance with VDOT and/or applicable Town standards and criteria or as otherwise established by the Town.
  - d. There shall be no buffer or reserve strips ("hate strips" or "spite strips") limiting access from existing or planned through streets, except under such limitations and conditions as may be in the form of a written approval from the Administrative Body. Such limitations and conditions in effect shall secure removal of the buffer or reserve

Design Standards

strip whenever it is in the public interest that such be converted into a public street as an access or additional access to adjacent lands.

12. Street signs and names
- a. At each street intersection within or adjacent to the proposed subdivision one street identification sign of a design approved by the Town shall be installed by, and at the expense of, the subdivider/developer. **Specifications for street name signage is located in Chapter 10 *Work on State Highways & Town Streets* of the Town of Berryville Construction Standards and Specifications Manual.**
  - b. The Town shall approve all new subdivision street names.
  - c. Proposed streets that are obviously in alignment with other already existing and named streets shall bear the name of the existing streets. In no case shall the names of proposed streets duplicate existing streets irrespective of the use of the suffix street, avenue, boulevard, drive, way, place, lane, or court.
13. Subdivision entrances
- a. No subdivision shall be approved unless the principle means of access thereto, and all streets within, shall conform to the standards of the Virginia Department of Transportation.
  - b. Each entrance onto any public road for vehicular traffic to and from such subdivision shall be subject to the approval of the Administrative Body upon the advice of the Virginia Department of Transportation Highway Engineer and shall be constructed in accordance with the applicable design standards of the Town of Berryville and/or the Virginia Department of Transportation.
  - c. Where traffic generated from a subdivision exceeds 2,000 vehicle-trips per day, such subdivision shall provide connectors to any existing public road(s) at two locations. Where only one connection is feasible, the entrance roadway must be a four-lane divided road with a length of not less than 250 feet or as otherwise recommended by the Virginia Department of Transportation. There shall be no curb cuts along this four-lane divided entrance road.
14. Alleys
- a. Dead-end alleys and alleys in residential areas will not be permitted.
  - b. Privately maintained and properly documented alley easements in commercial zones may be permitted at the discretion of the town, provided there is documentation acceptable to the Town ensuring the maintenance and upkeep of the alley easement. No such alley easement shall be less than 24 feet in width.
15. Service drives
- a. Whenever a proposed subdivision contains, or is adjacent to a minor or major arterial, sufficient land shall be reserved so as to provide for subsequent construction of service drives or service streets approximately parallel to such right-of-way; but the Administrative

Design Standards

Body may, when consistent with the public convenience and necessity, waive the requirement of such provision.

- b. Except where impractical by reason of topographic hardship, the area between the drive and the major highway shall be sufficient to provide for scenic planting and screening. The dimension of the area between the service drive and a major highway and the points of access between the same shall be determined after due consideration of traffic safety requirements.

C. **BLOCKS**

1. Block length  
The length of a block in a proposed subdivision shall be neither less than 400 feet nor more than 800 feet.
2. Block width  
The width of a proposed subdivision block shall be sufficient to allow two tiers of lots of minimum depth, except where fronting on arterial streets. The Administrative Body, upon recommendation by the Planning Director and/or Town Manager, may approve a single tier of lots of minimum depth wherever physiography or Berryville Area Master Plan matters so dictate.
3. Block orientation  
Where a proposed subdivision adjoins an arterial street or a collector street the Administrative Body may require service streets or driveways parallel to such arterial or collector street, or reverse frontage lots, to facilitate safe ingress or egress to the subdivision.
4. Nonconforming blocks  
Any proposed blocks of irregular shape or not conforming to the dimensions required in this Ordinance may be accepted upon special approval of the Administrative Body.

D. **LOTS**

1. Relationship to street  
Each lot shall abut on a street dedicated by the subdivision plat or deed of dedication, or on an existing public street.
2. Lot width and area  
The minimum width and area of a proposed subdivision lot shall be as set forth in the adopted Zoning Ordinance.
3. Lot frontage
  - a. Except for lots fronting on a cul-de-sac, frontage shall not be less than that required by the Zoning Ordinance. This regulation may be reduced for frontage on a public street or private road cul-de-sacs, provided that driveway separation shall be in accordance with Virginia Department of Transportation standards.
  - b. In establishing the required cul-de-sac lot frontage configurations, the minimum lot frontage as specified for any given zoning district shall be applied at the front setback line of the cul-de-sac lot.
4. Corner lots

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## DRAFT

Items for Street Signage	Manufacturer	Model #	Specifications
Standard public street name sign	N/A	N/A	MUTCD D3-1 3M Green background, white letters white rounded corners
Standard private street name sign	N/A	N/A	MUTCD D3-1 3M3430EGP3430 brown background, white letters white rounded corners
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Sign slot hardware	N/A	N/A	Standard sign 5.6" sign slot
Sign T slot hardware	N/A	N/A	5.6" t slot

**Streets and Utilities Committee Agenda Item Report Summary**

October 22, 2019

**Item Title**

Hermitage Phase V Street Name Change

**Prepared By**

Christy Dunkle

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**Background/History/General Information**

Town Council approved a rezoning of Phase V of the Hermitage subdivision in order to align proposed lots with zoning district lines. The modified lot layout is the result of new storm water management regulations established by the Virginia Department of Environmental Quality (DEQ). DEQ regulations now mandate that water quality be addressed through various best management practices in subdivision design. The new lot layout changed the street configuration and has resulted in Tyson Drive and Dunlap Drive existing within the same road casing. The original layout included a t-intersection. Staff has identified the need to determine the street name scenario.

**Findings / Current Activity**

Staff is recommending that the existing 13 lots addressed on Dunlap from the intersection of Hermitage Boulevard to the south be changed to Tyson Drive addresses. Staff has had a cursory discussion on this scenario with Clarke County E-911 staff and they indicated this is the best option.

**Financial Considerations**

N/A

**Schedule/Deadlines**

Forward to Town Council prior to the approval of the final plat for Hermitage Phase V. Please note this plat has not been submitted for review and approval.

**Other Considerations**

Committee input on other scenarios should be discussed.

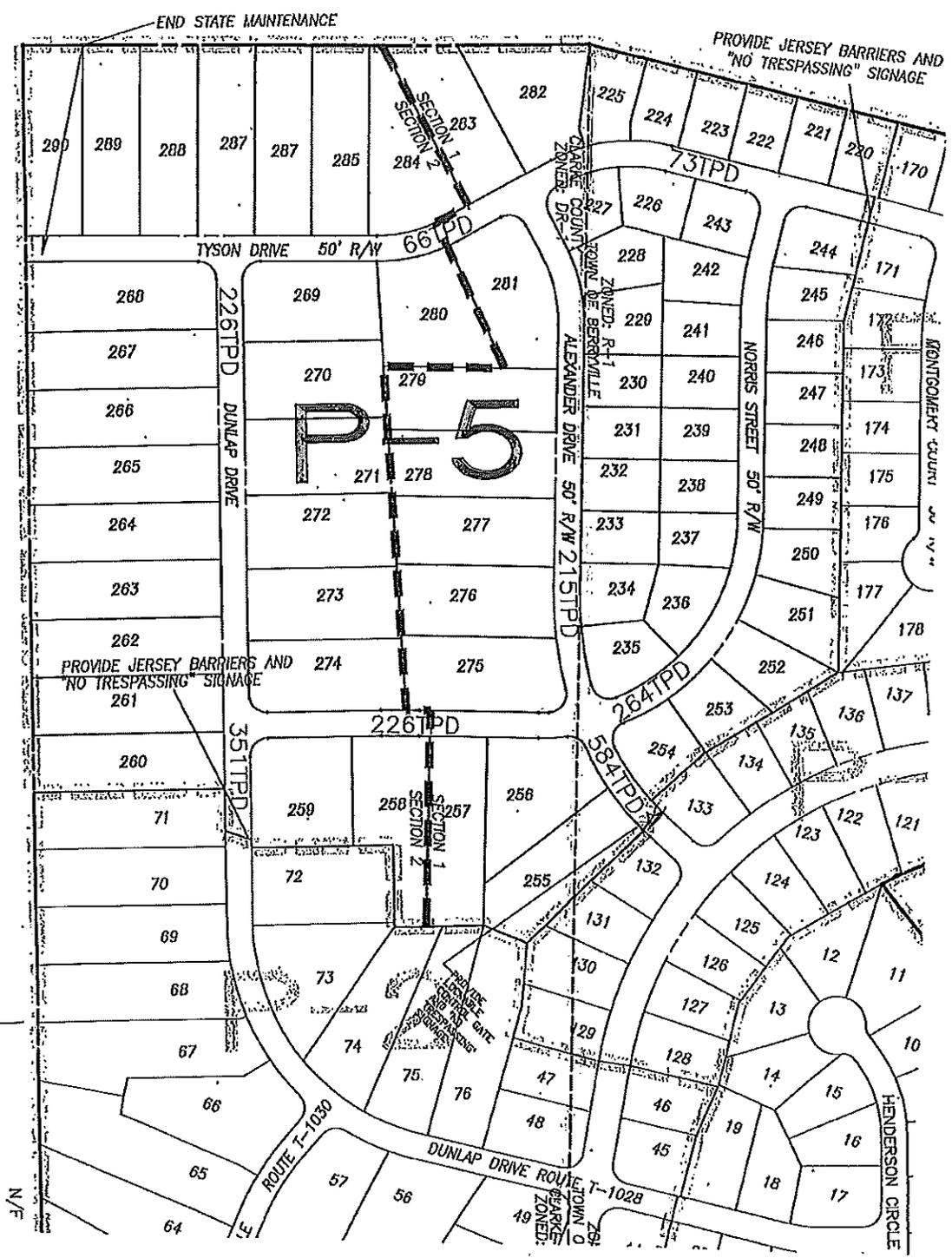
**Attachments**

- Original Phase V layout
- Updated Phase V layout
- Vicinity map identifying 13 existing lots in the area that would be affected by changing the name of Dunlap Drive to Tyson Drive from the southern intersection of Dunlap and Hermitage to the current end of Town maintenance.

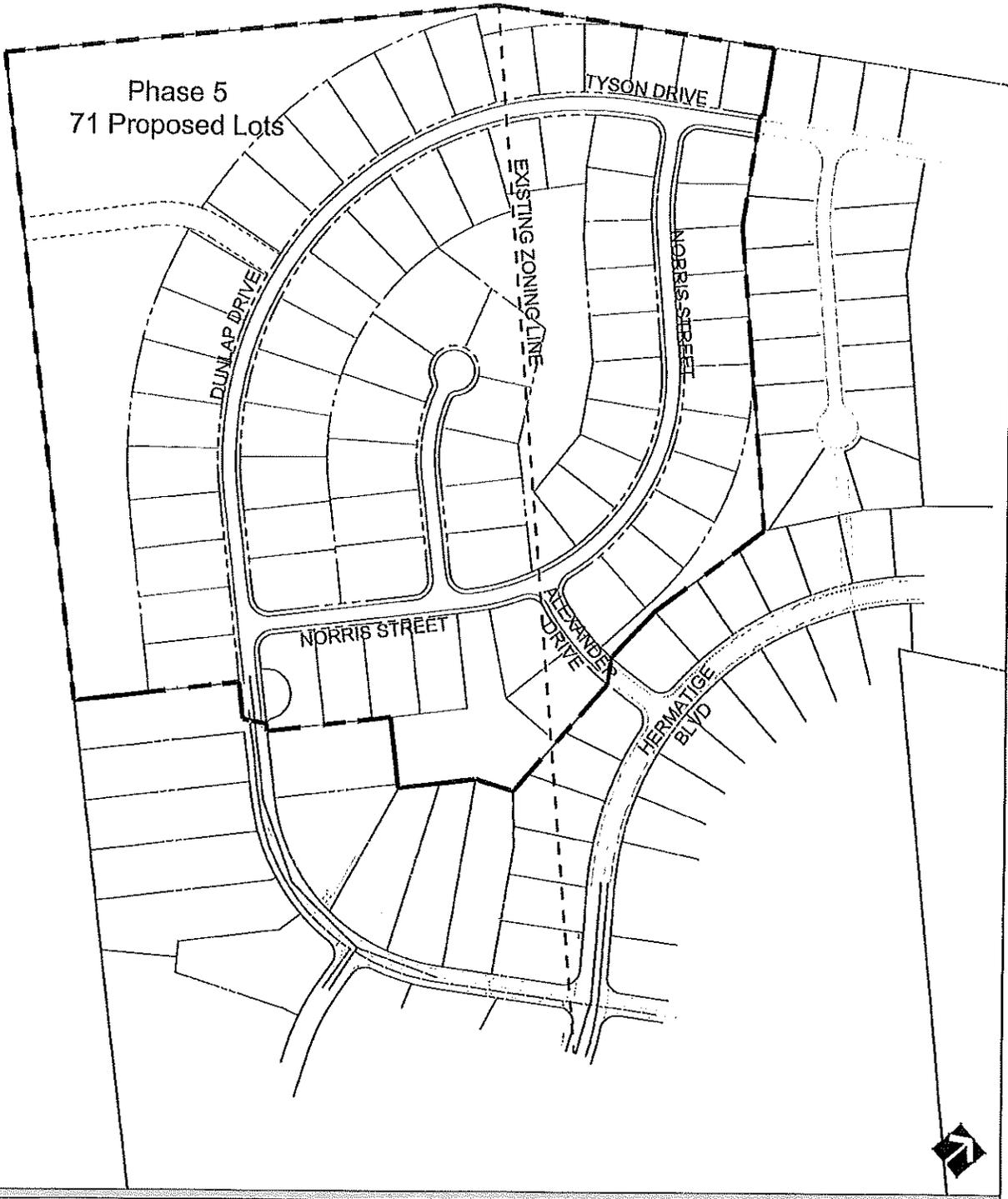
**Recommendation**

Determine a naming scenario and forward to Town Council for consideration.

# ORIGINAL PHASE V LAYOUT



# UPDATED PHASE V LAYOUT



## Hermitage Section 5, Exhibit A

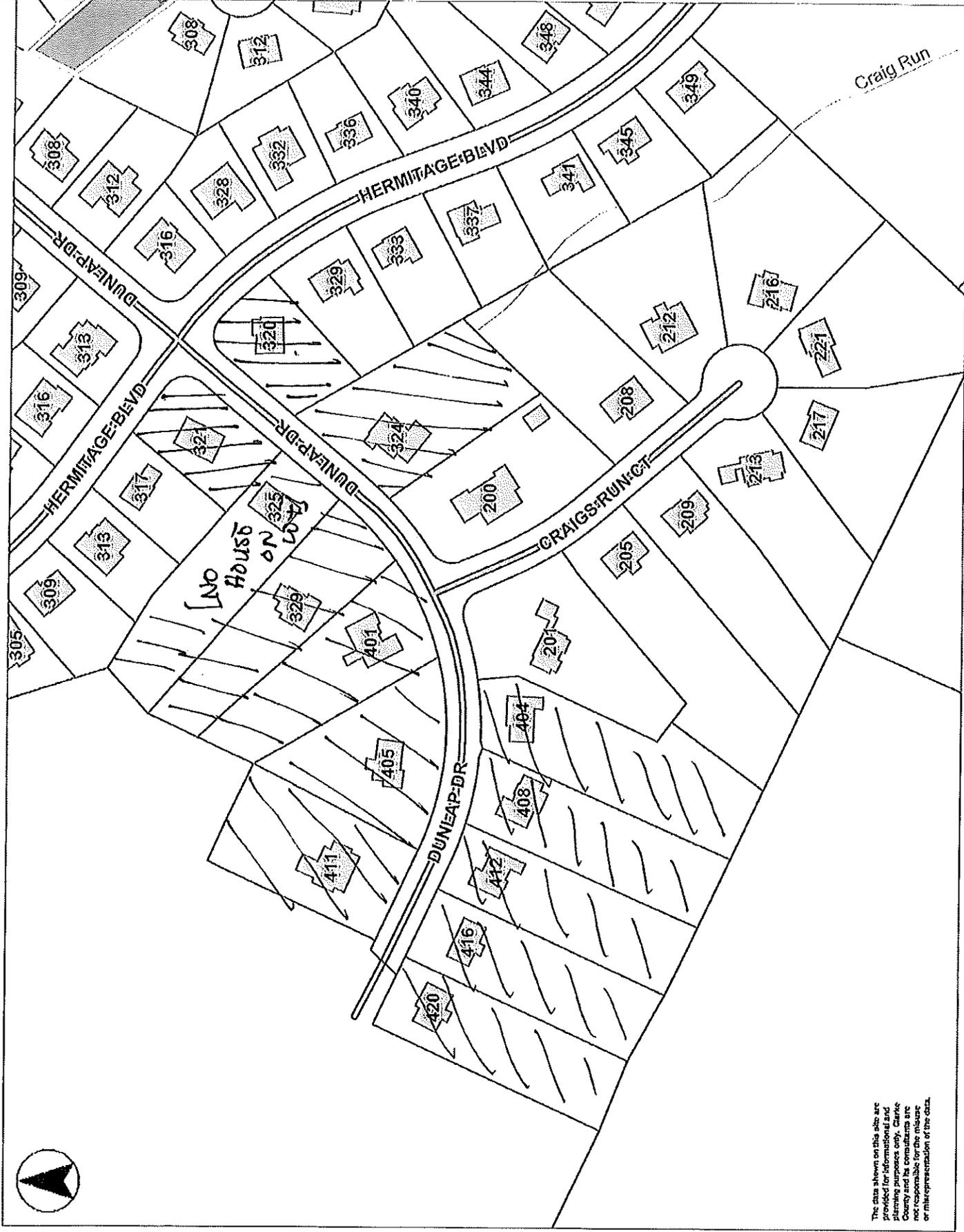
existing zoning line



PENNONI ENGINEERS, INC.



- Public
- Points of Interest
- Parcels
- Clarke County Boundary
- Major Roads
- Interstate
- US Highway
- State Highway
- Surrounding Counties Op
- Clarke County Roads
- Private Roads
- Roads
- Rail
- Buildings
- Appalachian Trail
- Streams
- Perennial Streams
- Intermittent Streams
- Ponds
- Rivers



# Clarke County MapsOnline

Printed on 10/18/2019 at 11:27 AM



The data shown on this site are provided for informational and planning purposes only. Clarke County and its employees are not responsible for the accuracy or misrepresentation of the data.



**No Attachment**

Other

# Closed Session

Adjourn